



## Initial Near-Miss Notification Report

**DATE**    22 May 09

- Navy Civilian Employee     Contract Employee     Military     Contractor's Employee

**1-Name of activity, installation, and exact location where incident occurred:**  
Camp Pendleton, Area 13, Building 13079

**2-Name of contractor, title of contract, and number of contract:**  
Dimensions Construction, Inc., Car Wash Addition 13 Area, N62473-08-C-3525

**3-Type of work being performed:** Erecting prefabricated concrete panels.

**4-Date and Time of incident:** May 20, 2009, 0645

**5-Brief description of the incident (see attached pictures):**

On May 19, 2009 eight prefabricated Waffle Crete panels were lifted into place and not braced in accordance with an engineered shoring plan. On the morning of May 20, 2009 work commenced for the day. A construction worker removed 4 pins that were connecting panels A and B in an attempt to realign an out of plumb panel. The minimal bracing that shored them up was not enough and they fell. Workers in the area had sufficient warning to get out of the way and no one was hit by the falling panels. The panels did hit adjacent panels causing some damage.

**6-Extent of property damage:** No Government property was damaged. At least two panels were demolished. Remaining panels will be inspected by a structural engineer. Structural engineer will forward report when complete. Estimated replacement cost to the contractor is unknown.

**7-Extent of injuries:** No injuries occurred.

**8-Employee immediate supervisor or responsible person:** Rodrigo Luna (QC, Site Superintendent & SSHO), Dimensions Construction, Inc.  
Kevin Parker (Foreman) Waffle-Crete.

**9-Immediate Actions taken:** All work was stopped and the site was secured. Upon notice ROICC field representative arrived on-site at 0715. ROICC Safety arrived on site at 0730.

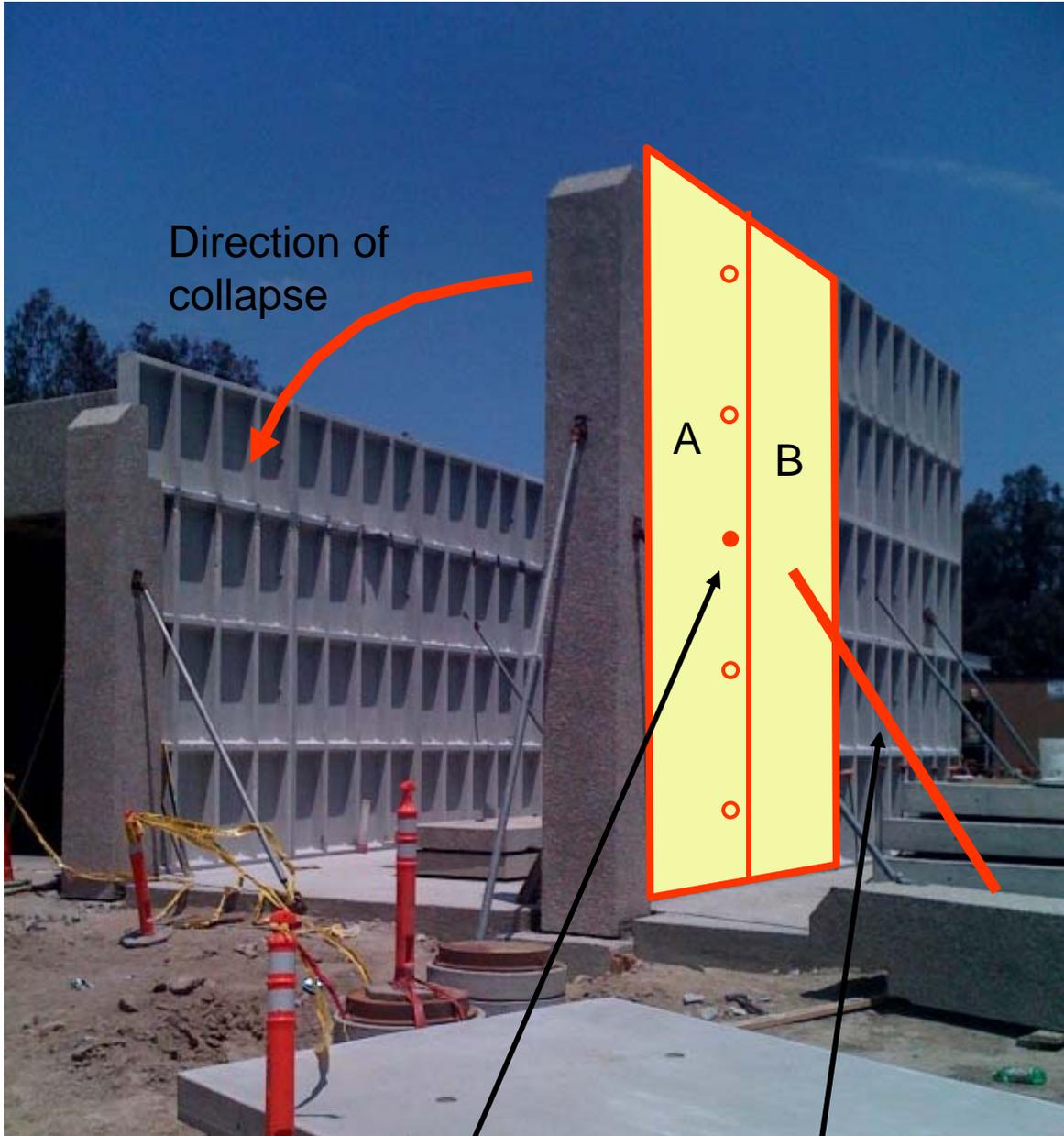
**10-Follow up Actions to be taken:** Contractor submitted an engineer approved shoring plan for Government review and acceptance. The government accepted the

plan on May 21, 2009. Contractor conducted a safety stand-down on May 21, 2009 at 0715 prior to resuming work. ROICC Safety, Andy Brochu, attended the safety stand-down and provided additional emphasis on our culture of safety. The topics covered were panel erection, shoring, bracing and anchor bolt installation. One of the erection crew members had a total disregard for safety procedures and has been removed from the job. The general contractor SSO/QC/Superintendent will be strictly enforcing the shoring plan and will personally monitor anchor bolt installation.

**11-Safety Investigators assigned:** None

**12-Initial Lessons learned:** Proper shoring and bracing is required during erection of precast concrete panels. An approved shoring plan must be developed specifically for each project and approved by an engineer. The shoring plan should be included in the AHA for any procedures involving lifting concrete panels. During the preparatory phase prior to starting a new sequence of work involving crane operations and concrete panel installation, the superintendent must review the shoring plan with the crew performing the work.

The attached pictures show existing conditions that led to the incident. The shop drawings show the engineer-approved shoring plan.



4 of 5 bolts removed.  
Middle bolt left in place to  
plumb panels

Only brace used to shore up two panels. When panel A  
began to tip, B was pulled down with it, pulling up the  
brace.

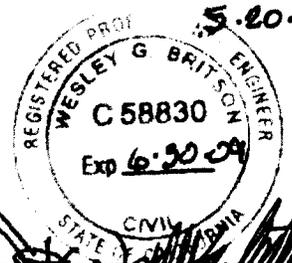
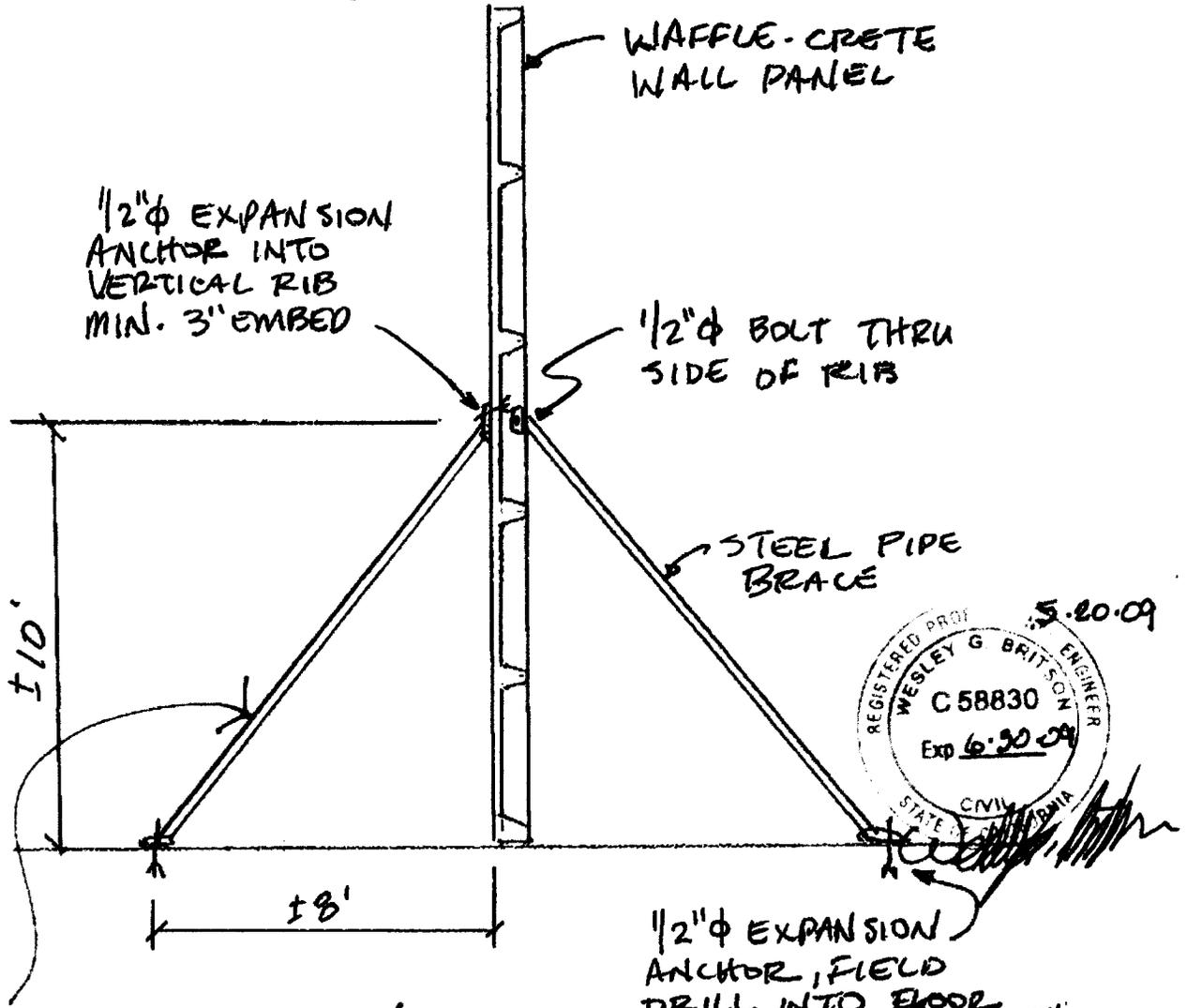


This was the only brace present on these two panels.  
Had the contractor followed the shoring plan, both panels  
would have been braced on both sides.

PANEL BRACING DETAILS

COLUMNS SIM.

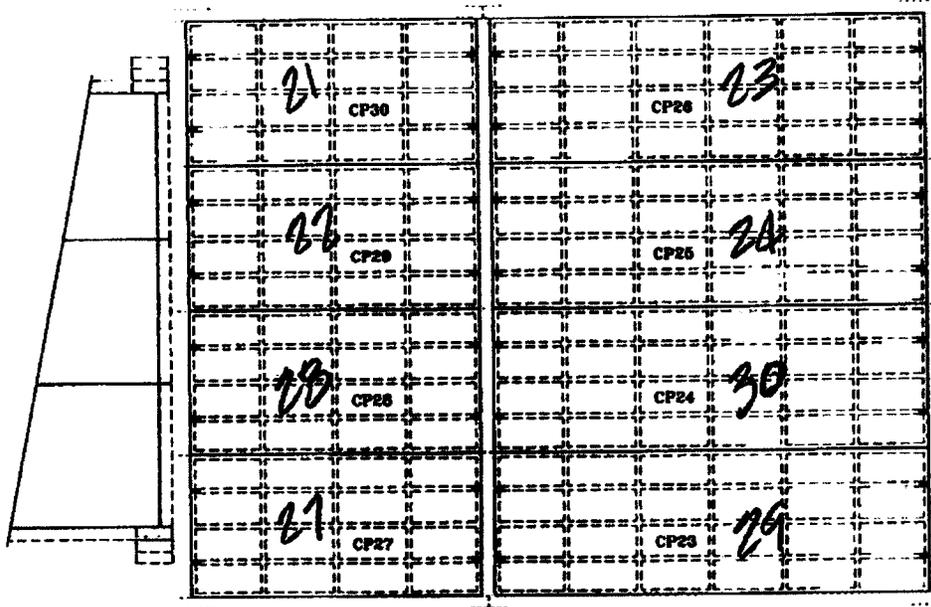
PANEL CANTILEVER OK.



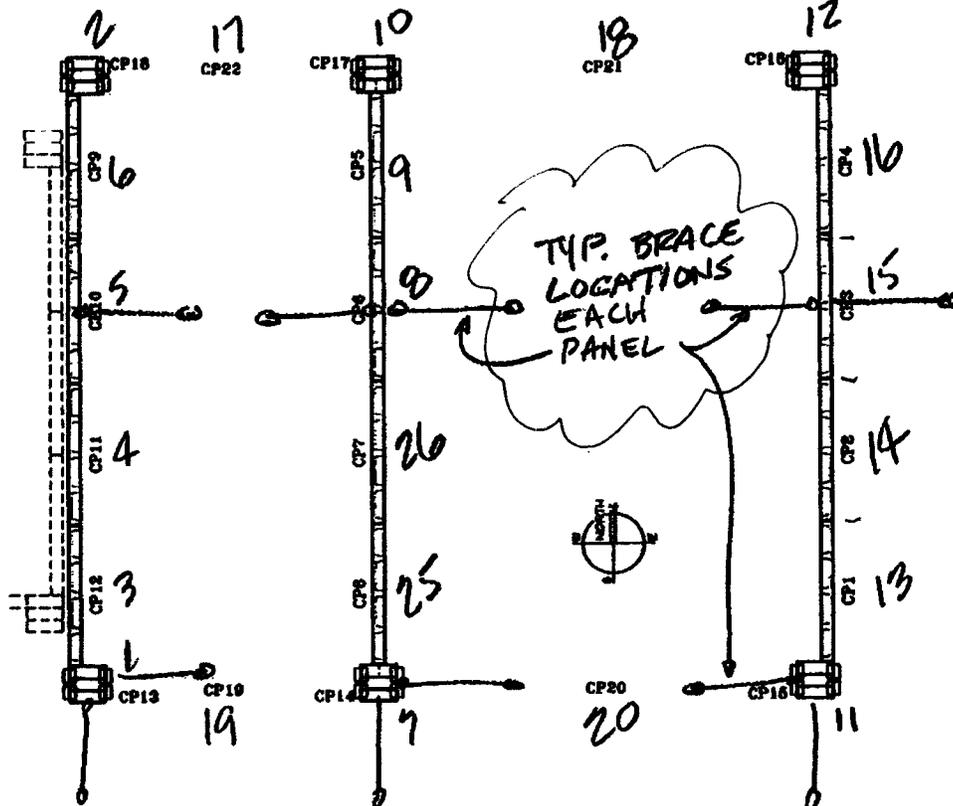
- VERIFY BRACE CAP. = 4<sup>K</sup> COMPRESSION SLAB, MIN. 3" 3 1/2" MIN. EMBEDMENT. REQ'D.
- VERIFY BOTH BRACES SIMULTANEOUSLY RESIST LOAD.
- VERIFY ANCHOR CAP. = 3<sup>K</sup> SHEAR IN SLAB ON GRADE.

— Stephen Andrews, P.E. (KS)

# ERECTION SEQUENCE



- 1 - CP13
- 2 - CP18
- 3 - CP12
- 4 - CP11
- 5 - CP10
- 6 - CP-9
- 7 - CP-14
- 8 - CP-6
- 9 - CP-5
- 10 - CP17
- 11 - CP-15
- 12 - CP-16
- 13 - CP-1
- 14 - CP-2
- 15 - CP-3
- 16 - CP-4
- 17 - CP-22
- 18 - CP-21
- 19 - CP-19
- 20 - CP-20
- 21 - CP-30
- 22 - CP-29
- 23 - CP-26
- 24 - CP-25
- 25 - CP-8
- 26 - CP-7
- 27 - CP-27
- 28 - CP-28
- 29 - CP-23
- 30 - CP-24



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Hays, Kansas USA  
Phone 785 625 3486

Camp Pendleton Car Wash Addition

JOB NUMBER: 2243  
DATE: 5/20/09  
DWN. BY: Rob D.  
CKD. BY:  
SCALE: as noted

REVISIONS:

1